

Item 4: Correspondence



REPLY TO
ATTENTION OF:

DEPARTMENT OF THE ARMY
BUFFALO DISTRICT, CORPS OF ENGINEERS
1776 NIAGARA STREET
BUFFALO, NEW YORK 14207-3199

July 27, 2015

Regulatory Branch

SUBJECT: Preliminary Jurisdictional Determination for Department of the Army Application
No. 2013-00443

Mr. Jamal Husani
Cuyahoga County
2079 East Ninth Street
Cleveland, Ohio 44115

Dear Mr. Husani:

I have reviewed the wetland delineation map you submitted for your proposal to perform runway safety improvements and modifications at the Cuyahoga County Airport – Robert D. Shea Field (CGF or Airport), which may result in placement of fill in waters of the U.S. (WOUS). CGF is located north of the intersection of Richmond Road and Highland Road, within the boundaries of three cities: Richmond Heights, Highland Heights, and Willoughby Hills, and crossing two counties: Cuyahoga and Lake County, Ohio.

I have evaluated your submitted wetland delineation map and have determined that the wetland and water boundaries shown on the map accurately represent on-site conditions. Please note that this is a Preliminary Jurisdictional Determination (JD). Preliminary JDs are non-binding written indications that there may be Waters of the United States (WOUS) on your parcel and approximate locations of those waters. Preliminary JDs are advisory in nature and may not be appealed.

Pursuant to Regulatory Guidance Letter 08-02, any permit application made in reliance on this Preliminary JD will be evaluated as though all wetlands or waters on the site are regulated by the Corps. Further, all waters, including wetlands will be used for purposes of assessing the area of project related impacts and compensatory mitigation. If you require a definitive response regarding Department of the Army jurisdiction for any or all of the waters identified on the submitted drawings, you may request an approved jurisdictional determination from this office. If an approved JD is requested, please be aware that this is often a lengthy process and we may require the submittal of additional information.

I have enclosed the Preliminary JD Form with this letter. The form and attached table identifies the extent of waters on the site and specific terms and conditions of the Preliminary JD. Please sign and return a copy of this form to my attention so that I may complete my evaluation of your file. If you do not respond within fifteen days of this letter, I will assume you no longer wish to pursue the jurisdictional determination and will withdraw your application.

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In accordance with Regulatory Guidance Letter 05-02, "Preliminary JDs are not definitive determinations of areas within regulatory jurisdiction and do not have expirations dates." However, I strongly recommend that the boundaries of WOUS be re-evaluated by a qualified wetland biologist after five years of the date of this letter. This will ensure that any changes are appropriately identified and you do not inadvertently incur a violation of Federal law while constructing your project or working on your project site.

Lastly, this determination has been conducted only to identify the limits of waters that may be subject to Corps Clean Water Act or Rivers and Harbors Act jurisdiction. This delineation/determination may not be valid for the wetland conservation provisions of the Food Security Act of 1985, as amended. If you or your tenant are USDA program participants, or anticipate participation in USDA programs, you should request a certified wetland determination from the local office of the Natural Resource Conservation Service prior to starting work.

A copy of this letter has been sent via email to Mr. Jason Earley of Lawhon & Associates, Inc., Mr. Luke Soposki of Lawhon & Associates, Inc., and to Mr. Simon Davies of CHA.

Questions pertaining to this matter should be directed to me at 716-879-4159, by writing to the following address: U.S. Army Corps of Engineers, 1776 Niagara Street, Buffalo, New York 14207, or by e-mail at: melissa.j.tarasiewicz@usace.army.mil

Sincerely,



Melissa Tarasiewicz
Biologist

Enclosures

Regulatory Branch
Subject: Preliminary Jurisdictional Determination for Department of the Army Application No.
2013-00443

ATTACHMENT

PRELIMINARY JURISDICTIONAL DETERMINATION FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PRELIMINARY JURISDICTIONAL DETERMINATION (JD): July 27, 2015

B. NAME AND ADDRESS OF PERSON REQUESTING PRELIMINARY JD:

Mr. Jamal Husani
Cuyahoga County
2079 East Ninth Street
Cleveland, Ohio 44115

C. DISTRICT OFFICE, FILE NAME, AND NUMBER: Buffalo, Cuyahoga County Airport- runway expansion, 2013-00443

**D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:
(USE THE ATTACHED TABLE TO DOCUMENT MULTIPLE WATERBODIES AT DIFFERENT SITES)**

State: Ohio County/parish/borough: Cuyahoga and Lake Counties
City: Richmond Heights, Highland Heights, and Willoughby Hills
Center coordinates of site (lat/long in degree decimal format): Lat. 41.56906° N, Long. -81.48684° W.

Universal Transverse Mercator: NAD 83

Name of nearest waterbody: East Branch of Euclid Creek

Identify (estimate) amount of waters in the review area:

Non-wetland waters: 7,066 linear feet; width (ft) varies (see attachment)

Cowardin Class: Riverine

Stream Flow: perennial, intermittent, ephemeral

Wetlands: 6.547 acres.

Cowardin Class: Palustrine forested, Palustrine emergent

Name of any water bodies on the site that have been identified as Section 10 waters: N/A

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

Office (Desk) Determination. Date: July 27, 2015

Field Determination. Date(s): April 29, 2015

Regulatory Branch

Subject: Preliminary Jurisdictional Determination for Department of the Army Application No. 2013-00443

1. The Corps of Engineers believes that there may be jurisdictional waters of the United States on the subject site, and the permit applicant or other affected party who requested this preliminary JD is hereby advised of his or her option to request and obtain an approved jurisdictional determination (JD) for that site. Nevertheless, the permit applicant or other person who requested this preliminary JD has declined to exercise the option to obtain an approved JD in this instance and at this time.

2. In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring "pre-construction notification" (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an approved JD for the activity, the permit applicant is hereby made aware of the following: (1) the permit applicant has elected to seek a permit authorization based on a preliminary JD, which does not make an official determination of jurisdictional waters; (2) that the applicant has the option to request an approved JD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an approved JD could possibly result in less compensatory mitigation being required or different special conditions; (3) that the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) that the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) that undertaking any activity in reliance upon the subject permit authorization without requesting an approved JD constitutes the applicant's acceptance of the use of the preliminary JD, but that either form of JD will be processed as soon as is practicable; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a preliminary JD constitutes agreement that all wetlands and other water bodies on the site affected in any way by that activity are jurisdictional waters of the United States, and precludes any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an approved JD or a preliminary JD, that JD will be processed as soon as is practicable. Further, an approved JD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331, and that in any administrative appeal, jurisdictional issues can be raised (see 33 C.F.R. 331.5(a)(2)). If, during that administrative appeal, it becomes necessary to make an official determination whether CWA jurisdiction exists over a site, or to provide an official delineation of jurisdictional waters on the site, the Corps will provide an approved JD to accomplish that result, as soon as is practicable. This preliminary JD finds that there "may be" waters of the United States on the subject project site, and identifies all aquatic features on the site that could be affected by the proposed activity, based on the following information:

Regulatory Branch

Subject: Preliminary Jurisdictional Determination for Department of the Army Application No. 2013-00443

SUPPORTING DATA. Data reviewed for preliminary JD (check all that apply

- checked items should be included in case file and, where checked and requested, appropriately reference sources below):

- Maps, plans, plots or plat submitted by or on behalf of the applicant/consultant: Lawhon & Associates, Inc.
- Data sheets prepared/submitted by or on behalf of the applicant/consultant.
 - Office concurs with data sheets/delineation report.
 - Office does not concur with data sheets/delineation report.
- Data sheets prepared by the Corps: Additional data points taken during the site visit April 29, 2015.
- Corps navigable waters' study:
- U.S. Geological Survey Hydrologic Atlas:
 - USGS NHD data.
 - USGS 8 and 12 digit HUC maps.
- U.S. Geological Survey map(s). Cite scale & quad name: 7.9 minute Mayfield Heights, OH.
- USDA Natural Resources Conservation Service Soil Survey. Citation: Soil Survey of Cuyahoga County and Lake County.
- National wetlands inventory map(s). Cite name: Mayfield Heights, OH.
- State/Local wetland inventory map(s):
- FEMA/FIRM maps:
- 100-year Floodplain Elevation is: (National Geodetic Vertical Datum of 1929)
- Photographs: Aerial (Name & Date): Bing Maps recent aerial imagery, Google Earth, 1991, 2001, 2003, 2004, 2005, 2006, 2007, 2009, 2010, 2011, 2012, 2014.
or Other (Name & Date): Photos submitted with the delineation report dated August 2013. Corps site visit photos dated April 2014.
- Previous determination(s). File no. and date of response letter:
- Other information (please specify):

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Melissa J. Tarasim 2/27/2015

Signature and date of
Regulatory Project Manager
(REQUIRED)

Signature and date of
person requesting preliminary JD
(REQUIRED, unless obtaining
the signature is impracticable)

Regulatory Branch
 Subject: Preliminary Jurisdictional Determination for Department of the Army Application No.
 2013-00443

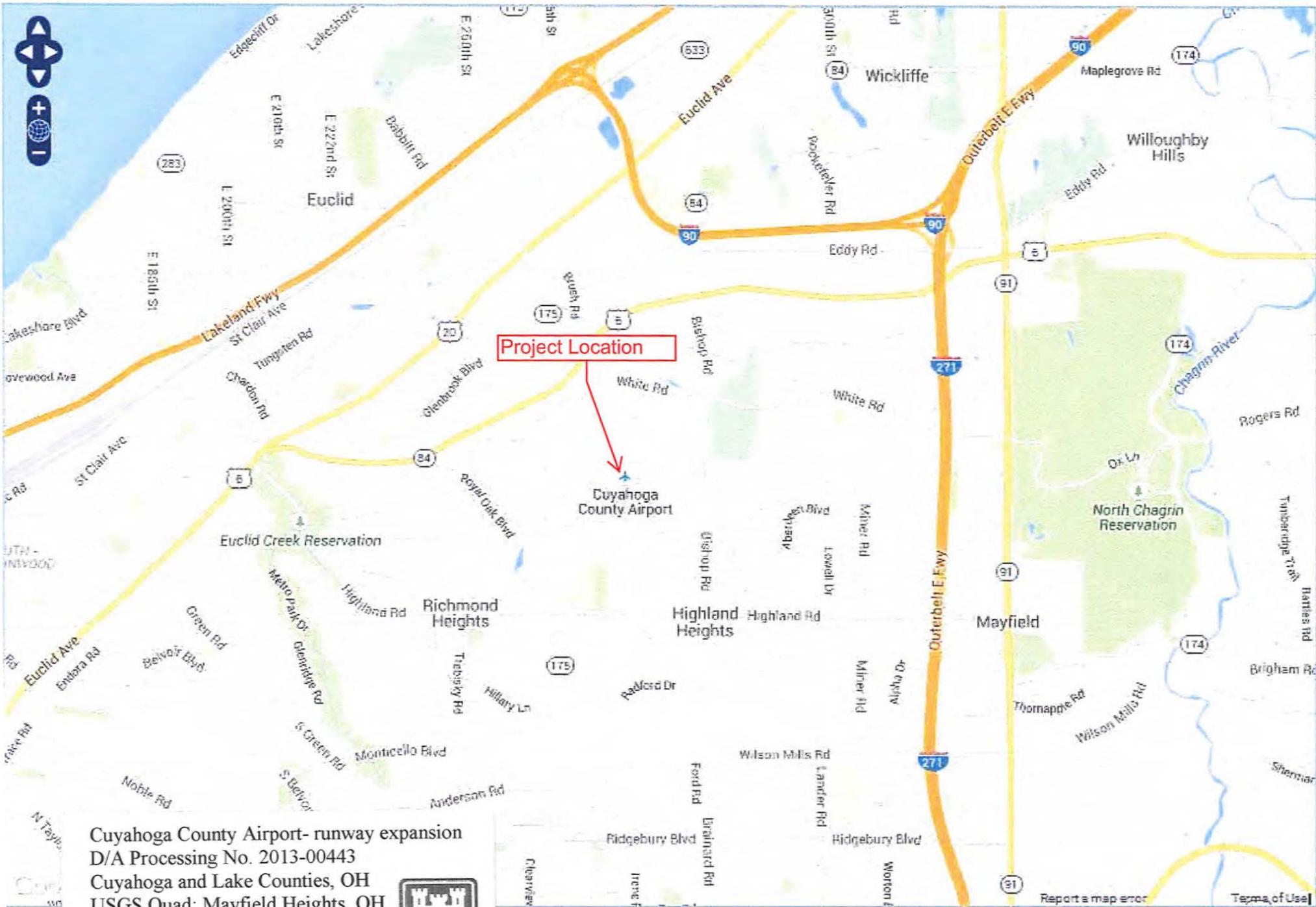
Aquatic Resource number	Latitude	Longitude	Cowardin Class	Estimated amount of aquatic resource in review area	Estimated stream width	Class of aquatic resource
Wetland E	41.56906	-81.48684	Palustrine emergent	0.476 acre (ac.)	-	Section 404 wetland
Wetland F	41.56906	-81.48684	Palustrine emergent	0.214 ac.	-	Section 404 wetland
Wetland G	41.56906	-81.48684	Palustrine emergent	1.764 ac.	-	Section 404 wetland
Wetland H	41.56906	-81.48684	Palustrine emergent	0.031 ac.	-	Section 404 wetland
Wetland I	41.56906	-81.48684	Palustrine emergent	0.075 ac.	-	Section 404 wetland
Wetland J	41.56906	-81.48684	Palustrine emergent	0.031 ac.	-	Section 404 wetland
Wetland K	41.56906	-81.48684	Palustrine emergent	0.613 ac.	-	Section 404 wetland
Wetland O	41.56906	-81.48684	Palustrine forested	0.812 ac.	-	Section 404 wetland
Wetland P	41.56906	-81.48684	Palustrine forested	0.182 ac.	-	Section 404 wetland
Wetland Q	41.56906	-81.48684	Palustrine forested	0.034 ac.	-	Section 404 wetland
Wetland R	41.56906	-81.48684	Palustrine forested	0.889 ac.	-	Section 404 wetland
Wetland S	41.56906	-81.48684	Palustrine emergent	0.091 ac.	-	Section 404 wetland
Wetland T	41.56906	-81.48684	Palustrine emergent	0.912 ac.	-	Section 404 wetland
Wetland U	41.56906	-81.48684	Palustrine emergent	0.054 ac.	-	Section 404 wetland
Wetland 1	41.56906	-81.48684	Palustrine emergent	0.259 ac.	-	Section 404 wetland
Wetland 2	41.56906	-81.48684	Palustrine emergent	0.053 ac.	-	Section 404 wetland
Wetland 3	41.56906	-81.48684	Palustrine emergent	0.044 ac.	-	Section 404 wetland

Regulatory Branch

Subject: Preliminary Jurisdictional Determination for Department of the Army Application No.

2013-00443

Wetland 4	41.56906	-81.48684	Palustrine emergent	0.015 ac.	-	Section 404 wetland
Ditch 2	41.56906	-81.48684	Riverine ephemeral	537 linear feet (LF)	1 foot (ft.)	Section 404 stream
Ditch 3	41.56906	-81.48684	Riverine ephemeral	511 LF	1 ft.	Section 404 stream
Ditch 4	41.56906	-81.48684	Riverine ephemeral	370 LF	1 ft.	Section 404 stream
Ditch 5	41.56906	-81.48684	Riverine ephemeral	462 LF	4 ft.	Section 404 stream
Ditch 8	41.56906	-81.48684	Riverine ephemeral	117 LF	2 ft.	Section 404 stream
Stream 1	41.56906	-81.48684	Riverine intermittent	191 LF	8 ft.	Section 404 stream
Stream 2	41.56906	-81.48684	Riverine intermittent	1,208 LF	8 ft.	Section 404 stream
Stream 3	41.56906	-81.48684	Riverine intermittent	499 LF	4 ft.	Section 404 stream
Stream 4	41.56906	-81.48684	Riverine ephemeral	707 LF	4 ft.	Section 404 stream
Stream 5	41.56906	-81.48684	Riverine perennial	848 LF	10 ft.	Section 404 stream
Stream 6	41.56906	-81.48684	Riverine perennial	203 LF	10 ft.	Section 404 stream
Stream 7	41.56906	-81.48684	Riverine perennial	498 LF	10 ft.	Section 404 stream
Stream 8	41.56906	-81.48684	Riverine perennial	43 LF	10 ft.	Section 404 stream
Stream 9	41.56906	-81.48684	Riverine intermittent	872 LF	8 ft.	Section 404 stream



Cuyahoga County Airport- runway expansion
 D/A Processing No. 2013-00443
 Cuyahoga and Lake Counties, OH
 USGS Quad: Mayfield Heights, OH
 Sheet 1 of 2



Report a map error Terms of Use



Extent Indicator Map

Legend

- JD Area
- Stream
- Ditch
- Catch basin
- Culvert
- Wetland

N

0 500 1,000
Feet

CUY-Airport

Ecological Resources Map
Full Extent

Lawton & Associates, Inc.

Date: July 2015	Approved by: JE	L&A No. 13-0166	Figure 3-1
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File Name: 130166_ecr_ark.mxd Date: 7/27/2015 By: dellano



U.S. Army Corps
Of Engineers
Buffalo District
CELRB-TD-R

Public Notice

Applicant: Cuyahoga
County Airport,
Department of Public
Works

Published: January 6, 2016
Expires: February 4, 2016

Application No: 2013-00443
Section: OH

All written comments should reference the above Application No. and be addressed to:
US Army Corps of Engineers, Buffalo District
Regulatory Branch (Attn:) Melissa Tarasiewicz
1776 Niagara Street
Buffalo, NY 14207

THE PURPOSE OF THIS PUBLIC NOTICE IS TO SOLICIT COMMENTS FROM THE PUBLIC REGARDING THE WORK DESCRIBED BELOW. NO DECISION HAS BEEN MADE AS TO WHETHER OR NOT A PERMIT WILL BE ISSUED AT THIS TIME.

**Application for Permit under Authority of
Section 404 of the Clean Water Act (33 U.S.C. 1344).**

APPLICANT: Cuyahoga County Airport, Department of Public Works

WATERWAY & LOCATION: Wetlands and streams adjacent to the East Branch of Euclid Creek in the Lake Erie watershed. The project site is located at the Cuyahoga County Airport (CGF) north of the intersection of Richmond Road and Highland Road, within the boundaries of three cities: Richmond Heights, Highland Heights, and Willoughby Hills, and crossing two counties: Cuyahoga and Lake Counties, Ohio

LATITUDE & LONGITUDE: Latitude North: 41.56447
Longitude West: -81.48742

EXISTING CONDITIONS:

Description of delineation of waters of the US: Eighteen (18) Palustrine emergent wetlands for a total of 6.55 acres (ac.); six (6) ephemeral streams for a total of 2,704 linear feet (LF); four (4) intermittent streams for a total of 2,770 LF; and four (4) perennial streams for a total of 1,592 LF were delineated on the subject site and determined to be federally jurisdictional.

PROPOSED WORK: The proposed project would include grading and filling eight (8) emergent wetlands for a total of 2.19 ac. of permanent wetland fill, and grading and filling four (4) ephemeral streams for a total of 1,279 LF of stream fill to extend Runway 6, install an Engineered Materials Arresting System (EMAS), repair runway and taxiway pavement, and install new drainage systems.

PROJECT PURPOSE:

Basic: Expand airport runway length, improve the quality of runway and taxiway surfaces, and improve airport runway safety.

Overall: Provide 5,500 feet of usable runway length, establish compliant runway safety areas pursuant to Federal Aviation Administration (FAA) requirements, and rehabilitate runway and taxiway pavement at the CGF airport.

Water Dependency Determination: The project is non-water dependent.

AVOIDANCE AND MINIMIZATION INFORMATION:

Preferred Alternative

A total of forty (40) project alternatives were considered. The Preferred Alternative (Alternative #23) was chosen because it meets the project's purpose and need while minimizing environmental impacts. Runway 6 would be extended 550 feet to the west and an EMAS would be installed. Runway 24 would be shortened by 110 feet to allow the EMAS to be installed. This alternative would provide 5,502 feet of pavement for takeoff operations in both directions with FAA compliant safety areas. When compared to the other project alternatives, Alternative #23 has the least anticipated impacts to floodplains, streams, and farmland. It does not impact parkland or recreational resources, has no road relocations, and has the least amount of proposed ground disturbance. Construction for Alternative #23 can be accomplished entirely on airport property.

The Preferred Alternative #23 would avoid impacts to higher quality wetlands located southwest and northeast of the airport by using the EMAS system and proposed runway extension distances to reduce the impact footprint. Impacts to degraded streams and wetlands within the existing airfield are proposed, and impacts to higher quality streams and wetlands adjacent to the airfield would be avoided. The stream and wetland fill proposed is limited to only that required to meet the FAA Runway Safety Area (RSA) design criteria. Excavated soil material will be placed only in upland locations and/or hauled off-site. Construction of 1,590 LF of new stream channel on-site with a 50 foot riparian buffer will minimize the direct impacts on the watershed by maintaining existing stream functions on-site. Avoidance of impacts to aquatic resources are somewhat constrained by the extent that open standing water can be allowed on airport property, which could attract wildlife and pose a safety issue for wildlife, people, and property.

Minimal Degradation Alternatives

Out of the 40 alternatives considered, the alternatives listed below would directly address runway safety area deficiencies and provide 5,500 feet of usable runway:

Alternative 15- Runway Reorientation (Relocate Bishop & Richmond Road)

Alternative 16- Runway 6 extension to the west (Relocate Richmond Road)

Alternative 17- Runway 24 extension to the east (Relocate Bishop Road)
Alternative 18- Runway 24 extension to the east (tunnel Bishop Road)
Alternative 23- EMAS at both Runway Ends (Preferred Alternative)
Alternative 24- Combination of Runway 24 shift to west and Runway 6 EMAS

Two of the alternatives (Alternatives #15 and #24) would result in fewer wetland impacts compared to the preferred alternative (Alternative #23). However, Alternatives #15 and #24 would result in greater impacts to the 100-year floodplain, streams, and farmland. The Preferred Alternative #23 has the least total disturbance and the least environmental impact.

Non-Degradation Alternatives

There are three potential non-degradation alternatives including the no-build, building an airport at a new location, or using another nearby airport alternatives.

No-Build Alternative

The proposed RSA improvements would not be brought up to FAA standards and the CGA airport would lose the potential to receive federal funding towards airport improvements including routine maintenance. Therefore, the airport would need to seek local or private funding, or close its operation.

Build a new airport at a new location Alternative

Development of a new site to replace the functions of CGF would likely involve substantial land acquisition, involve considerable residential and commercial property relocations, could take years to construct, and would result in substantial cost.

Use another nearby Airport

Three other airports within a 30-mile radius of CGF were considered as alternatives. The first airport, The Lost Nation Municipal Airport, does not have a runway of sufficient length to meet the project needs. The second airport, Burke Lakefront Airport (BKL), has existing infrastructure constraints and physical limitations to expansion, and it is unlikely that BKL would be able to absorb the tenants and aircraft operations from CGF. The third airport, Cleveland Hopkins International Airport, has the infrastructure to meet the project needs, but is focused on serving commercial airlines, and may not have the capacity to accept a significant number of general aviation operations. Additionally, relocating airport operations to another facility and abandoning the existing CGF infrastructure is not a practicable or feasible alternative since there is a demonstrated need to provide an airport to the local community.

PROPOSED MITIGATION: The applicant proposed to purchase 3.3 mitigation credits at the Cherry Valley mitigation bank through the Ohio Wetlands Preservation, Ltd. Additionally, 1,590 LF of new stream channel with a 50 foot riparian buffer would be constructed on-site.

Location and details of the above described work are shown on the attached maps and drawings.

Comments or questions pertaining to the work described in this notice should be reference the Application Number and be directed to the attention of Melissa Tarasiewicz, who can be contacted at the above address, by calling 716-879-4159, or by e-mail at: melissa.j.tarasiewicz@usace.army.mil A lack of response will be interpreted as meaning that there is no objection to the work as proposed.

The following authorization is required for this project:

Water Quality Certification (or waiver thereof) from the Ohio Environmental Protection Agency

Based on preliminary findings, there is one property adjacent to the federal permit area (Sheets 11-12 and 16-21 of 22, the Curtis-Wright hanger, that is eligible for inclusion in the National Register of Historic Places. This notice constitutes initiation of consultation with the Ohio Historic Preservation Office (SHPO) per Section 106 of the National Historic Preservation Act. All currently available historic resource information pertaining to this proposed project if any has been provided to the SHPO. Additional information concerning historic properties should be submitted to the Corps before the end of the comment period of this notice. The Corps will forward that information to the SHPO for their review.

Pursuant to Section 7 of the Endangered Species Act (16 U.S.C. 1531), the Corps of Engineers is consulting, under separate cover, with the USFWS to evaluate any potential impacts to: Indiana bat (*Myotis sodalis*), northern long-eared bat (*Myotis septentrionalis*), Kirtland's warbler (*Setophaga kirtlandii*), piping plover (*Charadrius melodus*), rufa red knot (*Calidris canutus rufa*), and snuffbox mussel (*Epioblasma triquetra*), and to ensure that the proposed activity is not likely to jeopardize their continued existence or result in the destruction or adverse modification of critical habitat.

This notice is promulgated in accordance with Title 33, Code of Federal Regulations, parts 320-330. Any interested party desiring to comment on the work described herein may do so by submitting their comments, in writing, so that they are received no later than 4:30 pm on the expiration date of this notice.

Comments submitted in response to this notice will be fully considered during the public interest review for this permit application. All written comments will be made a part of the administrative record which is available to the public under the Freedom of Information Act. The Administrative Record, or portions thereof may also be posted on a Corps of Engineers internet web site. Due to resource limitations, this office will normally not acknowledge the receipt of comments or respond to individual letters of comment.

Any individual may request a public hearing by submitting their written request, stating the specific reasons for holding a hearing, in the same manner and time period as other comments.

Public hearings for the purposes of the Corps permit program will be held when the District Commander determines he can obtain additional information, not available in written comments, that will aid him in the decision making process for this application. A Corps hearing is not a source of information for the general public, nor a forum for the resolution of issues or conflicting points of view (witnesses are not sworn and cross examination is prohibited). Hearings will not be held to obtain information on issues unrelated to the work requiring a permit, such as property ownership, neighbor disputes, or the behavior or actions of the public or applicant on upland property not regulated by the Department of the Army. Information obtained from a public hearing is given no greater weight than that obtained from written comments. Therefore, you should not fail to make timely written comments because a hearing

might be held.

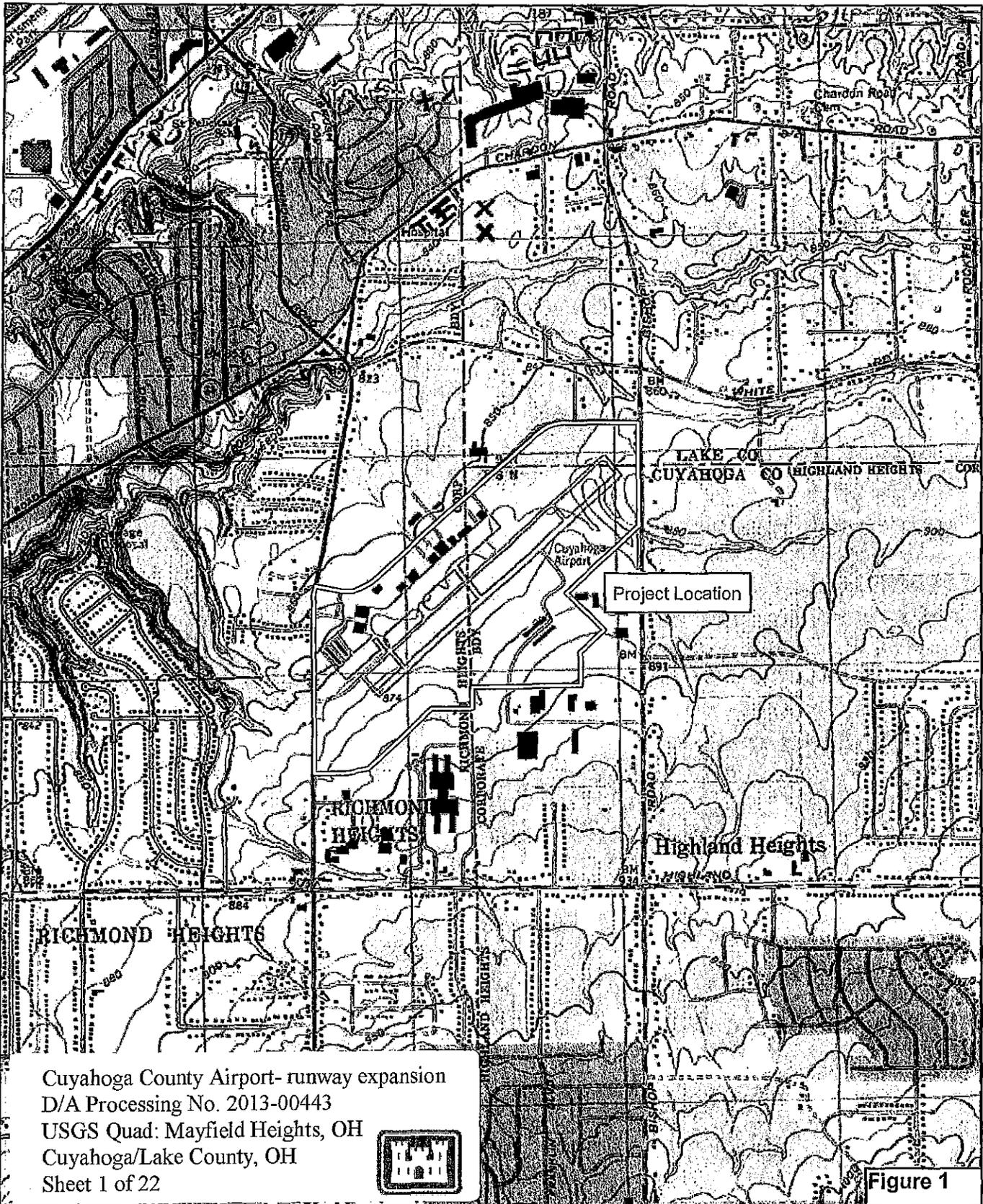
The decision to approve or deny this permit request will be based on an evaluation of the probable impact, including cumulative impacts of the proposed activity on the public interest. That decision will reflect the national concern for both protection and utilization of important resources. The benefits which reasonably may be expected to accrue from the proposal must be balanced against its reasonably foreseeable detriments. All factors which may be relevant to the proposal will be considered including the cumulative effects thereof; among these are conservation, economics, aesthetics, general environmental concerns, wetlands, historic properties, fish and wildlife values, flood hazards, flood plain values, land use, navigation, shoreline erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food and fiber production, mineral needs, considerations of property ownership, and in general, the needs and welfare of the people.

The Corps of Engineers is soliciting comments from the public; Federal, state and local agencies and officials; Indian Tribes; and other interested parties in order to consider and evaluate the impacts of this proposed activity. Any comments received will be considered by the Corps of Engineers to determine whether to issue, modify, condition or deny a permit for this proposal. To make this decision, comments are used to assess impacts on endangered species, historic properties, water quality, general environmental effects, and the other public interest factors listed above. Comments are used in the preparation of an Environmental Assessment and/or an Environmental Impact Statement pursuant to the National Environmental Policy Act. Comments are also used to determine the need for a public hearing and to determine the overall public interest of the proposed activity.

SIGNED

Diane C. Kozlowski
Chief, Regulatory Branch

NOTICE TO POSTMASTER: It is requested that this notice be posted continuously and conspicuously for 30 days from the date of issuance.



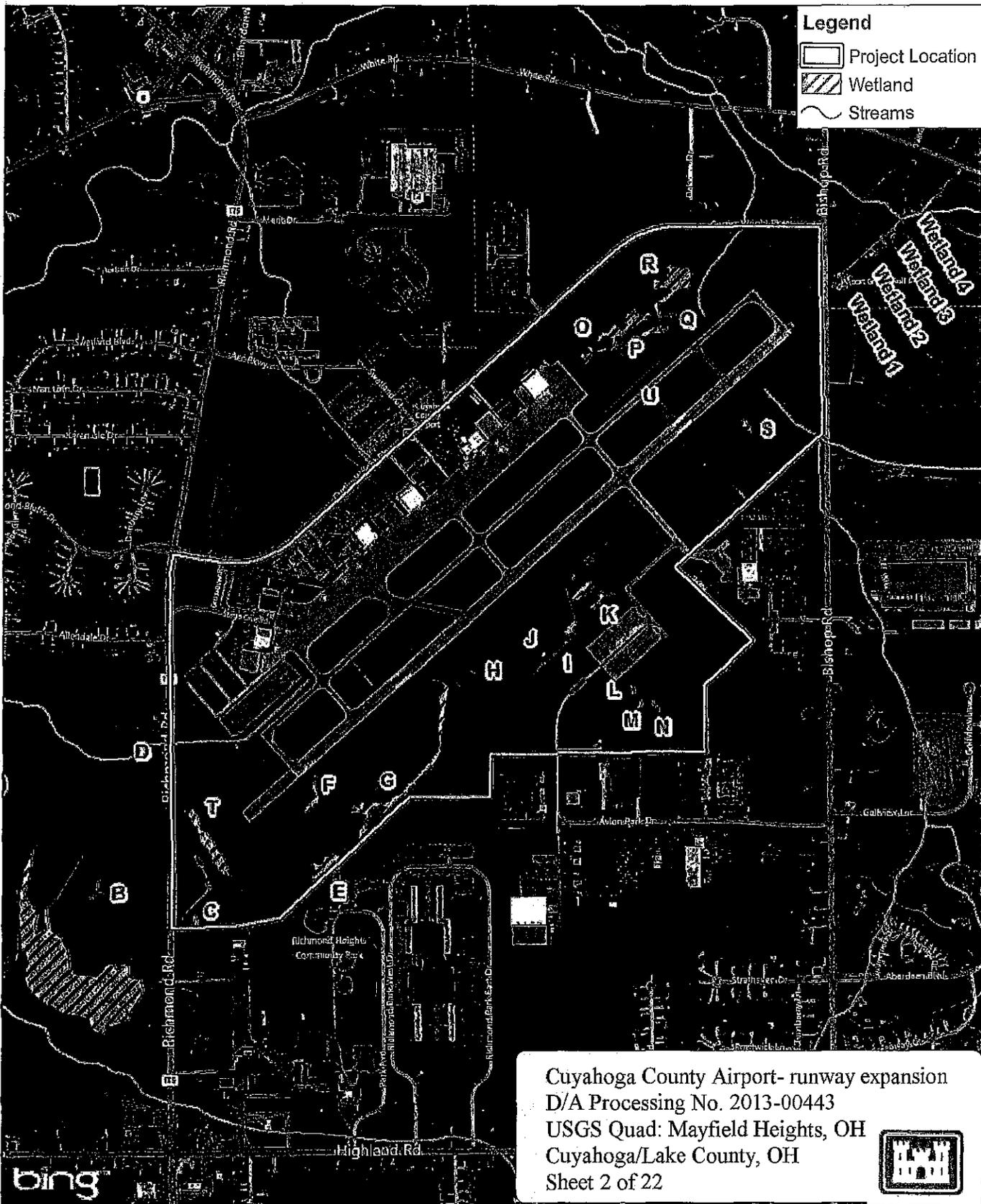
CHA
 design/construction solutions

0 1,000 2,000 Feet
 Scale 1" = 2000'

CHA No.
 27986

USGS Topographic Map
 Cuyahoga County Airport
 Runway 6/24
 Safety Area Improvements
 Richmond Heights, Cuyahoga County, Ohio

Service Layer Credits:
 Copyright: © 2013 National Geographic Society, I-cubed
 Mayfield Heights USGS Quadrangle Date: 1996



Cuyahoga County Airport- runway expansion
 D/A Processing No. 2013-00443
 USGS Quad: Mayfield Heights, OH
 Cuyahoga/Lake County, OH
 Sheet 2 of 22



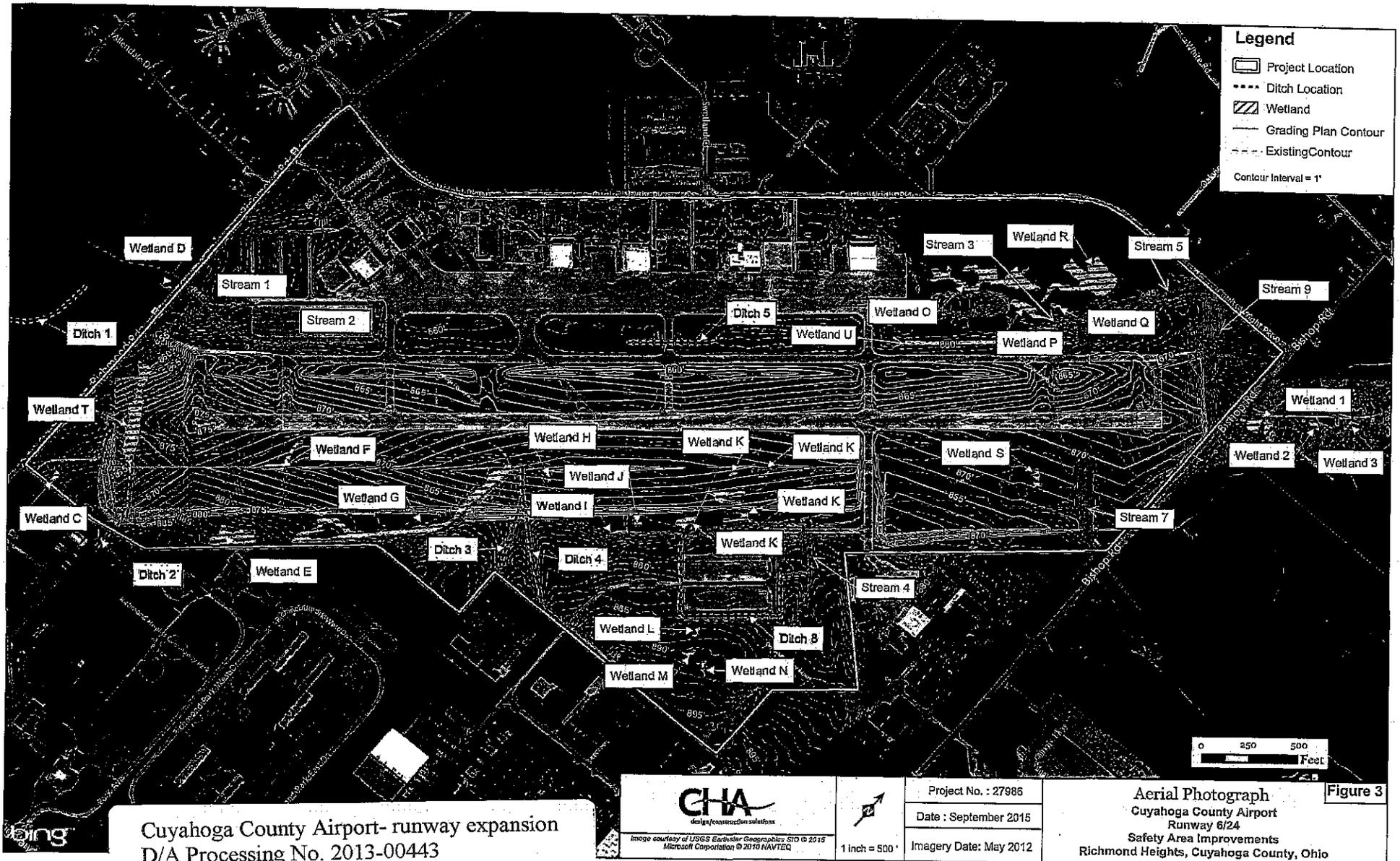

CHA
 design/construction solutions

0 500 1,000
 Feet
 Scale 1" = 1000'

CHA Project No.
 27986

Vicinity Map
 Cuyahoga County Airport
 Runway 6/24
 Safety Area Improvements
 Richmond Heights, Cuyahoga County, Ohio

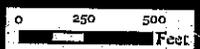
Image courtesy of USGS Earthstar Geographics SIO © 2015
 Microsoft Corporation © 2010 NAVTEQ
 Photo Date: 5/15/2012



Legend

- Project Location
- Ditch Location
- Wetland
- Grading Plan Contour
- Existing Contour

Contour Interval = 1'



CHA
design/construct solutions

Image courtesy of USGS Earthstar Geographics STD © 2015
Microsoft Corporation © 2010 NAVTEQ

1 inch = 500'

Project No. : 27985
Date : September 2015
Imagery Date: May 2012

Figure 3

Aerial Photograph
Cuyahoga County Airport
Runway 6/24
Safety Area Improvements
Richmond Heights, Cuyahoga County, Ohio

Cuyahoga County Airport- runway expansion
D/A Processing No. 2013-00443
USGS Quad: Mayfield Heights, OH
Cuyahoga/Lake County, OH
Sheet 3 of 22



Figure 2.1 Alternative 15 – Runway Reorientation (Relocate Bishop and Richmond Road)

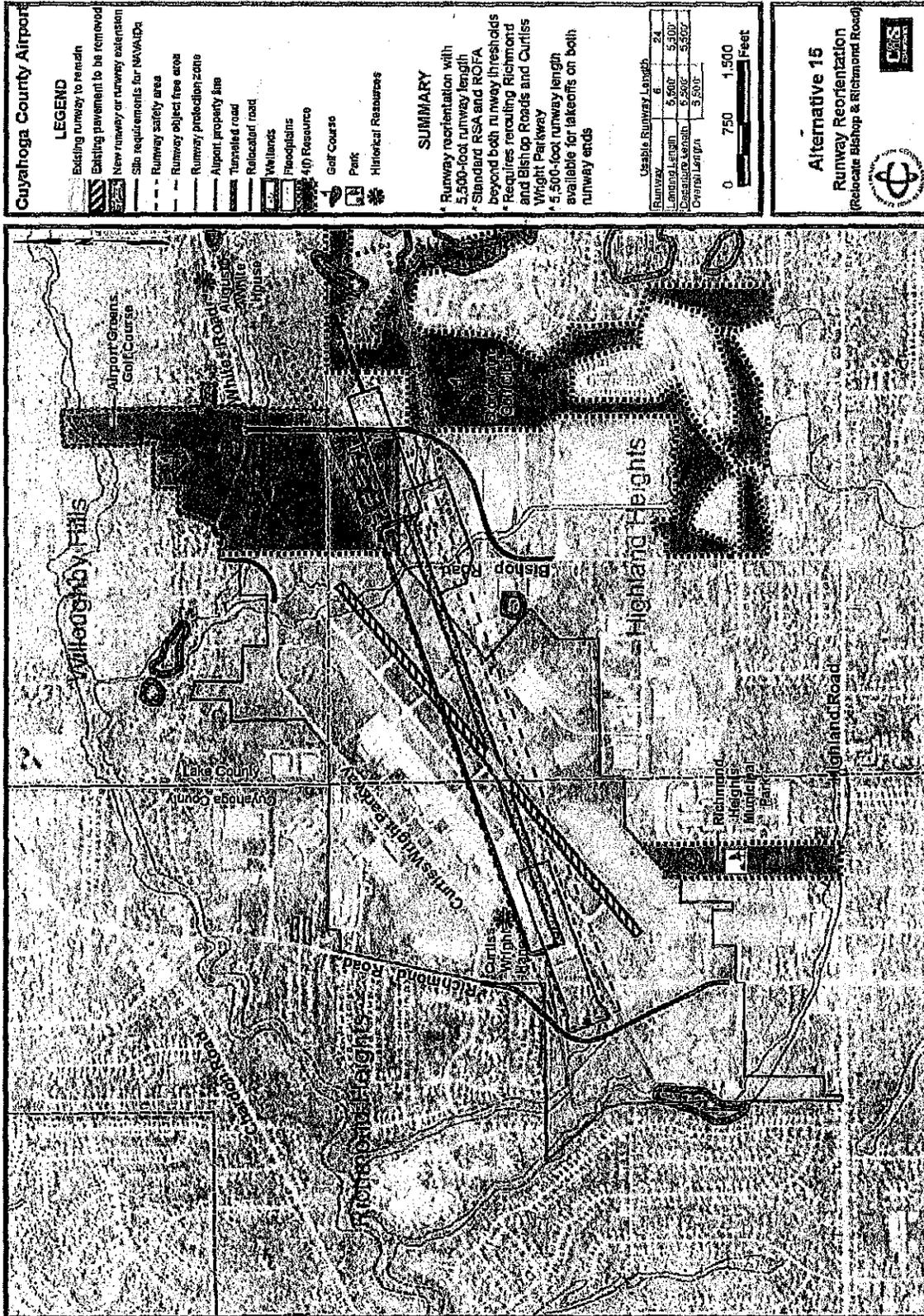


Figure 2.4 Alternative 18 – Runway 24 Extension to the East (Tunnel Bishop Road)

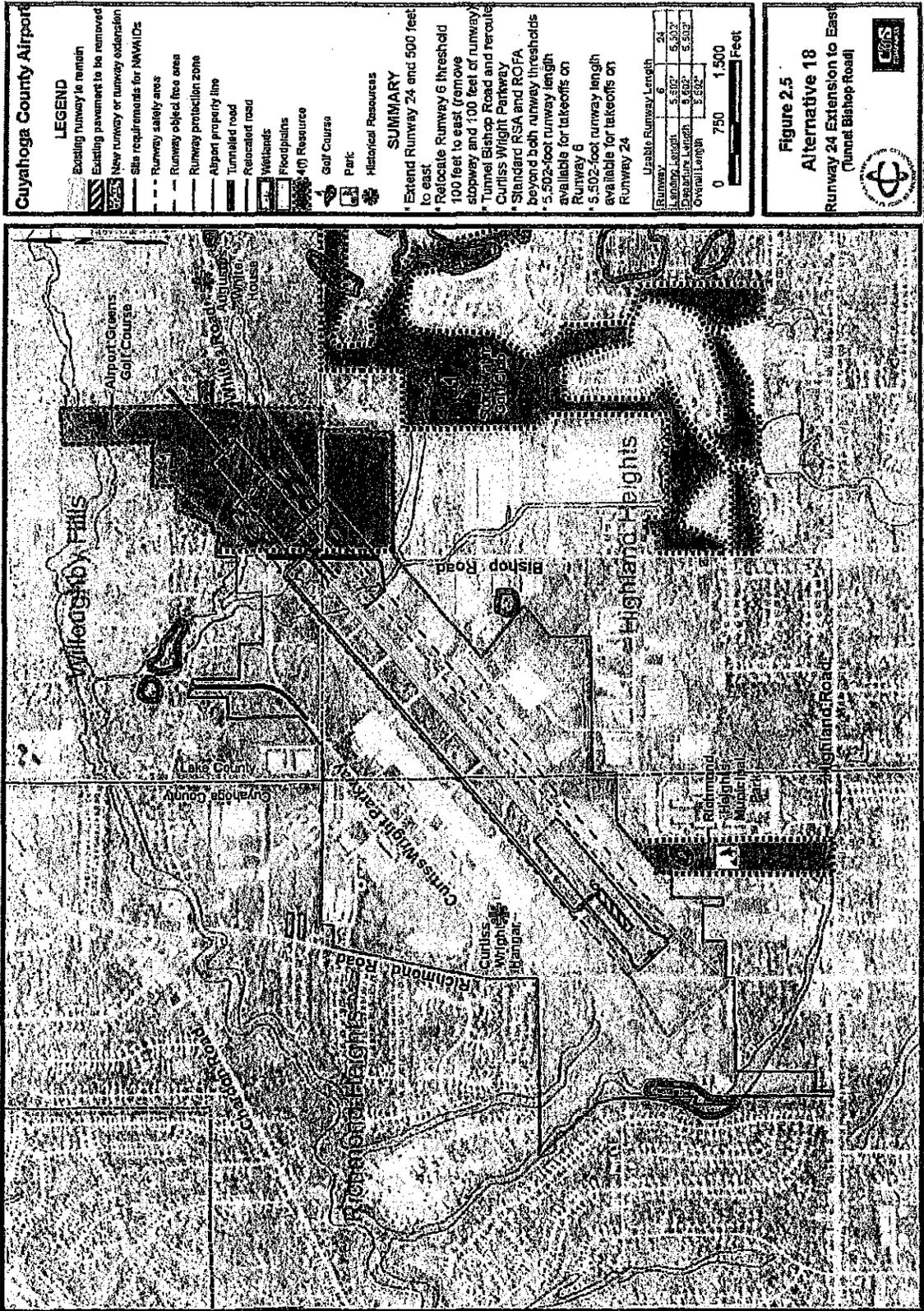


Figure 2.5 Alternative 19 – Road Relocations at Both Runway Ends

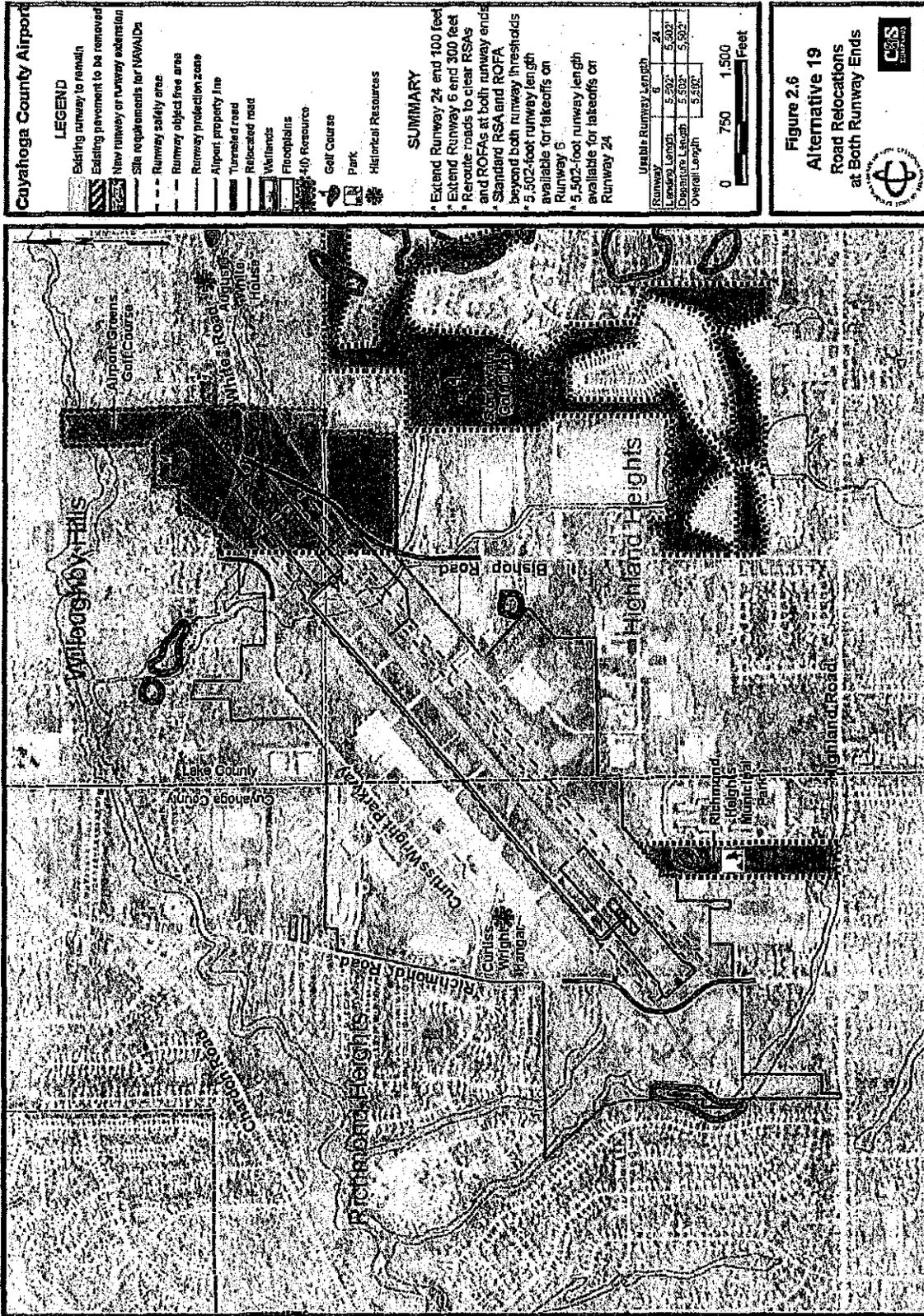
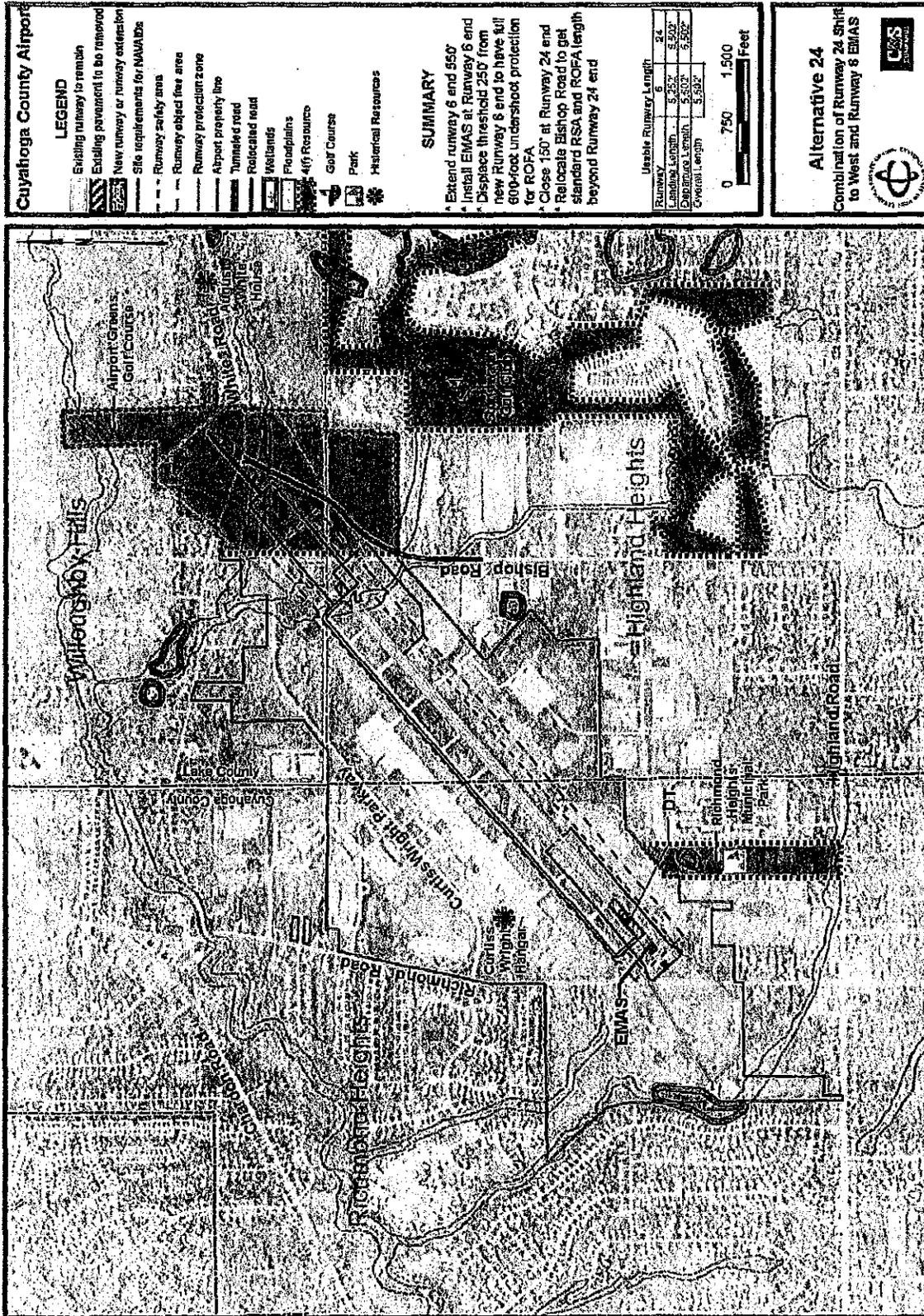


Figure 2.7 Alternative 24 – Combination of Runway 24 Shift to West and Runway 6 EMAS



Cuyahoga County Airport- runway expansion
 D/A Processing No. 2013-00443
 USGS Quad: Mayfield Heights, OH
 Cuyahoga/Lake County, OH
 Sheet 12 of 22



	CUYAHOGA COUNTY AIRPORT SAFETY AREA IMPROVEMENTS RUNWAY 8/24 GRADING PLAN - 2	Scale: As Shown Project No.: 2013-00443 Date: 12/15/13	Check Tag Date: 12/15/13 Checked By: [Signature]		RICHMOND HEIGHTS, OH 44135 CUYAHOGA COUNTY AIRPORT 5200 CUYAHOGA WING PARKWAY	1/8" = 1'-0" 1/4" = 3'-0" 1/2" = 6'-0" 3/4" = 9'-0" 1" = 12'-0" 1 1/4" = 15'-0" 1 1/2" = 18'-0" 1 3/4" = 21'-0" 2" = 24'-0" 2 1/4" = 27'-0" 2 1/2" = 30'-0" 2 3/4" = 33'-0" 3" = 36'-0" 3 1/4" = 39'-0" 3 1/2" = 42'-0" 3 3/4" = 45'-0" 4" = 48'-0" 4 1/4" = 51'-0" 4 1/2" = 54'-0" 4 3/4" = 57'-0" 5" = 60'-0" 5 1/4" = 63'-0" 5 1/2" = 66'-0" 5 3/4" = 69'-0" 6" = 72'-0" 6 1/4" = 75'-0" 6 1/2" = 78'-0" 6 3/4" = 81'-0" 7" = 84'-0" 7 1/4" = 87'-0" 7 1/2" = 90'-0" 7 3/4" = 93'-0" 8" = 96'-0" 8 1/4" = 99'-0" 8 1/2" = 102'-0" 8 3/4" = 105'-0" 9" = 108'-0" 9 1/4" = 111'-0" 9 1/2" = 114'-0" 9 3/4" = 117'-0" 10" = 120'-0" 10 1/4" = 123'-0" 10 1/2" = 126'-0" 10 3/4" = 129'-0" 11" = 132'-0" 11 1/4" = 135'-0" 11 1/2" = 138'-0" 11 3/4" = 141'-0" 12" 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D/A Processing No. 2013-00443
 USGS Quad: Mayfield Heights, OH
 Cuyahoga/Lake County, OH
 Sheet 13 of 22



APPROVED FOR CONTRACT	DATE

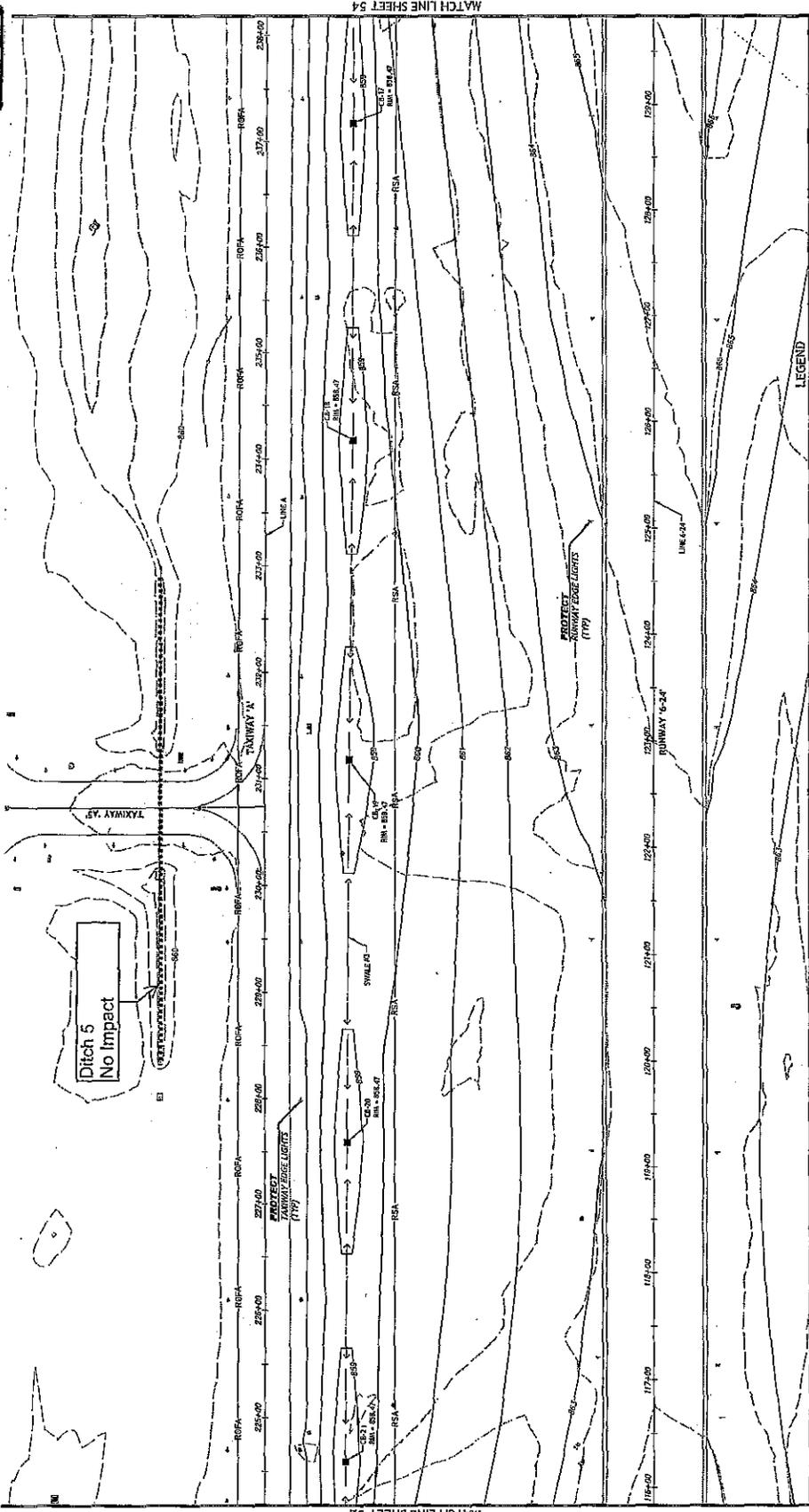
CUYAHOGA COUNTY AIRPORT
 2800 CURTIS WRIGHT PARKWAY
 RICHMOND HEIGHTS, OH 44143



CHVA
 1801 W. 12th Street
 Cleveland, OH 44115
 Phone: (216) 421-1212
 Fax: (216) 421-1213

CUYAHOGA COUNTY AIRPORT
 RUNWAY 6/24
 SAFETY AREA IMPROVEMENTS
 GRADING PLAN - 3
 Scale: AS NOTED
 Project No. 2788

53

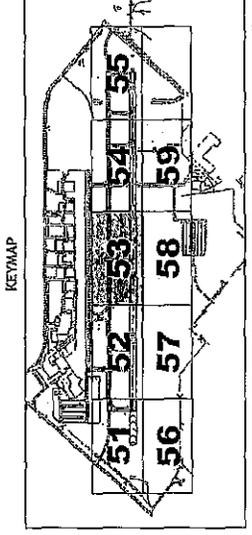


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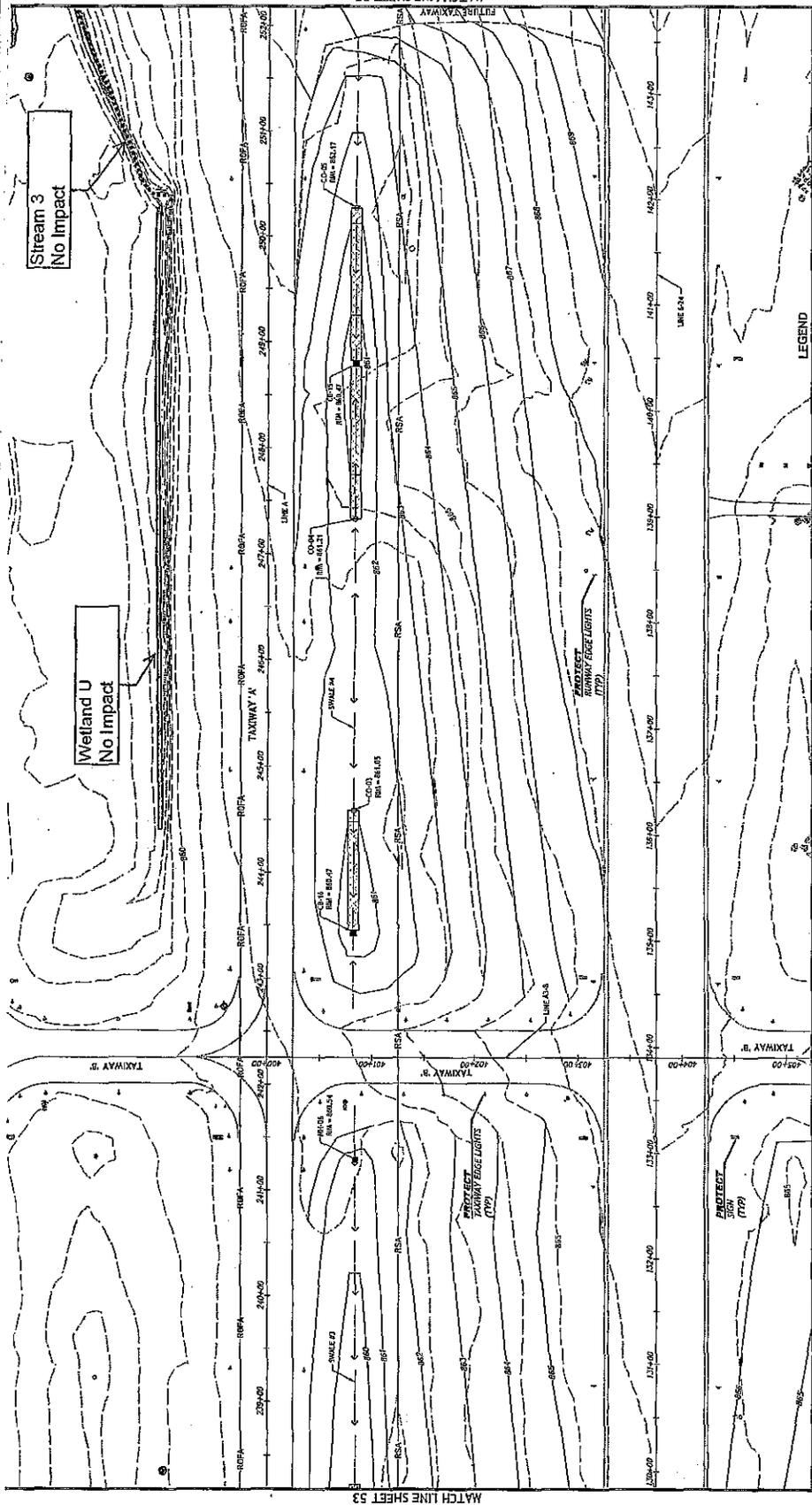
- PROPOSED SIDEWALL
- PROPOSED BARRIERS
- PROPOSED CATCHBASIN
- PROPOSED CLEARANCE
- PROPOSED RUNWAY LIGHT
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- PROPOSED RUNWAY LIGHT
- PROPOSED SAFETY AREA
- PROPOSED SAFETY AREA
- PROPOSED SAFETY AREA
- PUTURE AIRFIELD PAVEMENT
- MARKING ALTERNATE BID #1 LIMITS
- MARKING ALTERNATE BID #1 LIMITS

GRAPHIC SCALE (FEET)
 0 25 50 100

- NOTES
1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES AND AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES AND AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES AND AGENCIES.
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 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES AND AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES AND AGENCIES.
 9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES AND AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL AFFECTED AGENCIES AND AGENCIES.
 10. IF ALTERNATE BID #1 IS NOT AWARDED SET THE CROSS SECTION FOR THE PROPOSED TRANSITION BACK TO THE ORIGINAL.



D/A Processing No. 2013-00443
 USGS Quad: Mayfield Heights, OH
 Cuyahoga/Lake County, OH
 Sheet 14 of 22



LEGEND

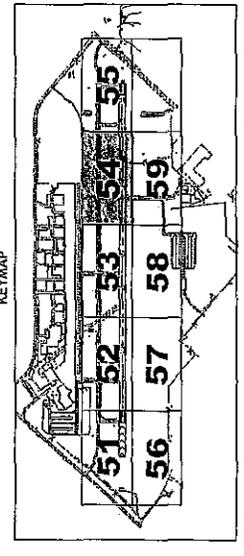
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- PROPOSED CATCHBAY
- PROPOSED CLEARCUT
- PROPOSED TAXIWAY LIGHT
- PROPOSED RUNWAY LIGHT
- PROPOSED RUNWAY SAFETY AREA
- RSMA
- ROFA
- RUNWAY OBJECT FREE AREA
- RUNWAY IMPROVED PAVEMENT
- FUTURE FORESTATION CELL
- EXISTING FORESTATION CELL

GRAPHIC SCALE (FEET)

0 25 50 75 100

NOTES

- THE EXISTING CONDITIONS SHOWN ON THIS DRAWING ARE DERIVED FROM MULTIPLE AERIAL AND FIELD SURVEYS. PRIOR TO THE START OF THE CONTRACTOR ACTIVITIES, THE CONTRACTOR SHALL VERIFY THE BOUNDS OF ANY DISCREPANCIES IN THE EXISTING INFORMATION.
- UNDESIGNED UTILITIES ARE INDICATED FROM AVAILABLE INFORMATION, BUT ARE NOT KNOWN TO BE ACCURATE OR COMPLETE. IF ANY UNDESIGNED UTILITIES ARE ENCOUNTERED DURING CONSTRUCTION, IMMEDIATELY NOTIFY THE ENGINEER.
- ALL WORK SHALL BE SECURED AND PROTECTED BY CONE AND FLAG TO PREVENT ALIEN INTERFERENCE. ALL WORK SHALL BE SECURED AND PROTECTED BY CONE AND FLAG TO PREVENT ALIEN INTERFERENCE.
- THE CONTRACTOR SHALL MAINTAIN ALL EXISTING UTILITIES AND ALL UNDERGROUND UTILITIES SHALL BE MAINTAINED AND PROTECTED THROUGHOUT THE PROJECT.
- THE PROPOSED CONTOUR LINES ARE THE RESULT OF AN ENGINEER'S GROUND SURVEY AND SHALL BE A MAJOR INPUT WITH REGARD TO THE DESIGN OF THE RUNWAY OR TAXIWAY. ANY WORKS WITHIN THE CONTINUITY OF THE CHANNEL OR CONTIGUOUS AREAS SHALL BE MAINTAINED THROUGHOUT THE PROJECT.
- THE EXISTING CONDITIONS SHOWN ON THIS DRAWING ARE DERIVED FROM MULTIPLE AERIAL AND FIELD SURVEYS. PRIOR TO THE START OF THE CONTRACTOR ACTIVITIES, THE CONTRACTOR SHALL VERIFY THE BOUNDS OF ANY DISCREPANCIES IN THE EXISTING INFORMATION.
- ALL WORK SHALL BE SECURED AND PROTECTED BY CONE AND FLAG TO PREVENT ALIEN INTERFERENCE.
- IF ANY DISCREPANCIES ARE FOUND, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY.



CUYAHOGA COUNTY AIRPORT SAFETY AREA IMPROVEMENTS GRADING PLAN - 4

Scale AS NOTED

Project No. 2013-00443

Checked: DPF
 Drawn: RBN
 Date: 12/14/13

Checked: TWS
 Drawn: RBN
 Date: 12/14/13

1000 Y
 1:1000
 STATE OF OHIO
 ENGINEERING DIVISION
 EXPIRES 12-31-15

28300 CURTIS WRIGHT PARKWAY
 CUYAHOGA COUNTY AIRPORT
 RICHMOND HEIGHTS, OH 44115

No. 54



NO.	DATE	BY	REVISION

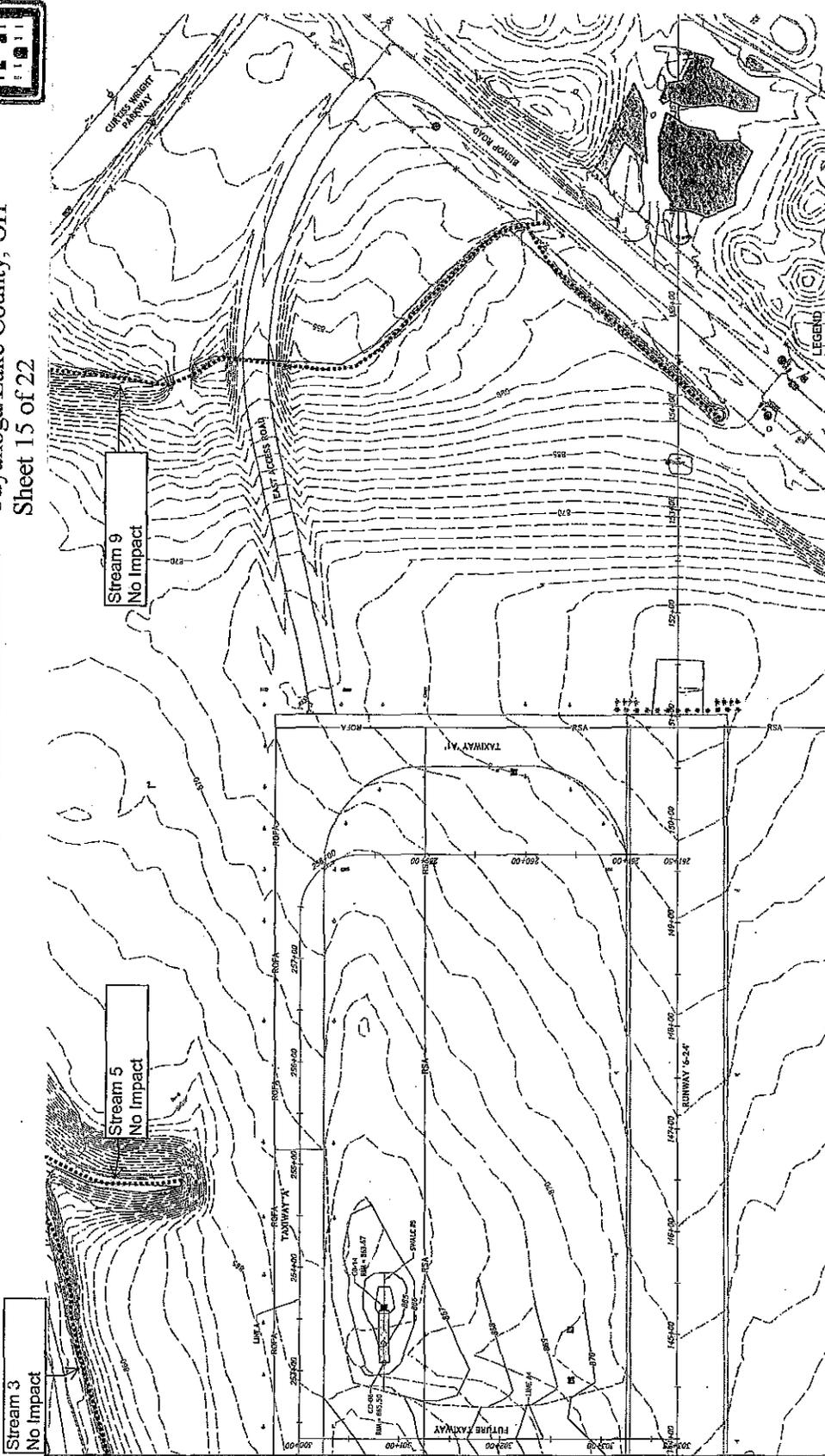
CUYAHOGA COUNTY AIRPORT
 26300 COURTS WRIGHT PARKWAY
 RICHMOND HEIGHTS, OH 44143

STATE OF OHIO
 DEPARTMENT OF PUBLIC SAFETY
 DIVISION OF PROFESSIONAL ENGINEERING

GHA
 GEOTECHNICAL & HYDROLOGICAL ASSOCIATES
 15100 WOODBURN ROAD
 CLEVELAND, OH 44130
 PHONE: (216) 751-1100
 FAX: (216) 751-1101
 WWW.GHA-INC.COM

CUYAHOGA COUNTY AIRPORT
 SAFETY AREA IMPROVEMENTS
 RUNWAY B24
 GRADING PLAN - 5

55



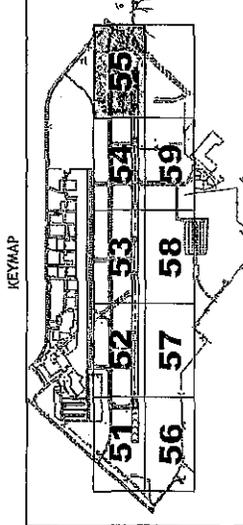
LEGEND

- PROPOSED RECEIVAL
- PROPOSED MANHOLE
- PROPOSED CIRCULAR
- PROPOSED CLEANOUT
- PROPOSED TAXIWAY LIGHT
- PROPOSED RUNWAY LIGHT
- RUNWAY SAFETY AREA
- RUNWAY OBJECT FREE AREA
- FUTURE AIRFIELD PARKWAY
- ADJUSTED IDENTIFICATION CELL
- ADJUSTIVE ALTERNATE ID #1 LIMITS

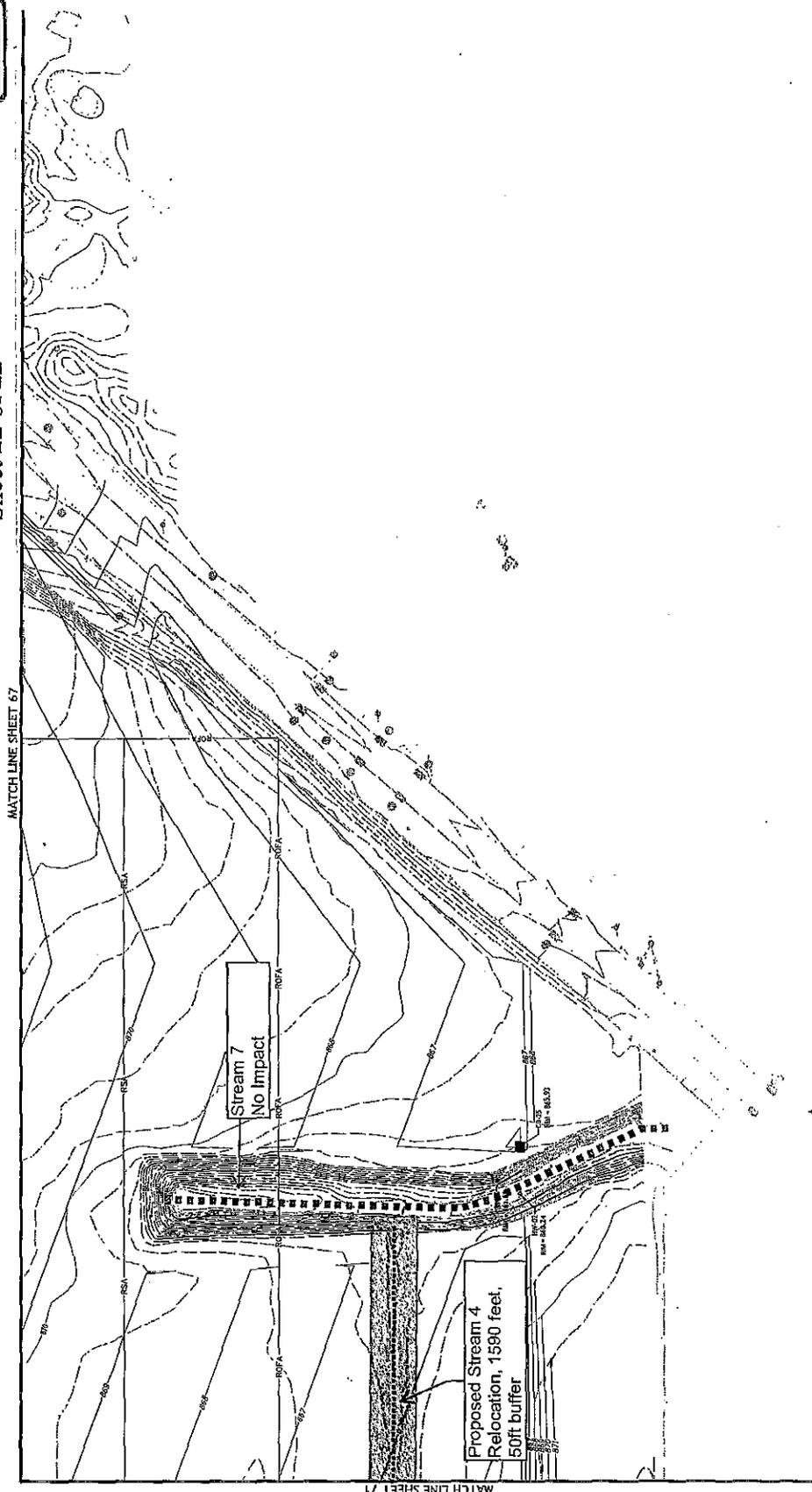
GRAPHIC SCALE (FEET)
 0 30 60

NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND RIGHTS OF WAY PRIOR TO THE START OF CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL NOTIFY THE OWNER OF ANY DISCREPANCIES IN THE EXISTING INFORMATION REFLECTED IN THIS PLAN.
2. ALL DIMENSIONS ARE SHOWN FROM THE CENTERLINE UNLESS OTHERWISE NOTED. DIMENSIONS ARE NOT TO BE CONSIDERED AS A GUARANTEE OF ACCURACY.
3. ACTIVE UNOCCUPIED UNDEVELOPED LANDS ARE UNOCCUPIED UNDEVELOPED LANDS. THESE ARE NOT TO BE CONSIDERED AS A GUARANTEE OF ACCURACY.
4. UNOCCUPIED UNDEVELOPED LANDS ARE UNOCCUPIED UNDEVELOPED LANDS. THESE ARE NOT TO BE CONSIDERED AS A GUARANTEE OF ACCURACY.
5. THE PROPOSED CONSTRUCTION IS THE RESULT OF AN ENGINEER'S JUDGMENT AND IS BASED ON THE INFORMATION PROVIDED TO THE ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED TO THE ENGINEER.
6. THE CONTRACTOR SHALL NOT BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED TO THE ENGINEER.
7. THESE SHALL BE THE SLOPE WITHIN THE RSA GREATER THAN 1:1. AT THE COMPLETION OF EACH NIGHT AND WEEKEND GRADING ACTIVITY THE CONTRACTOR SHALL NOT BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED TO THE ENGINEER.
8. IF ADJUSTIVE ALTERNATE ID #1 IS NOT ADOPTED, THE CROSS SECTION FOR THE PROPOSED TRANSITION BACK TO EXISTING GRADE.



Cuyahoga County airport-railway expansion
 D/A Processing No. 2013-00443
 USGS Quad: Mayfield Heights, OH
 Cuyahoga/Lake County, OH
 Sheet 22 of 22



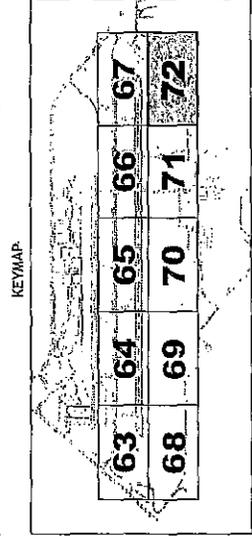
Stream 7
No Impact

Proposed Stream 4
Relocation, 1590 feet,
50ft buffer

LEGEND

- HEADWALL
- ▭ ALIQUOT
- CUTBANK
- TRUNK LIGHT
- RUNWAY LIGHT
- RUNWAY SAFETY AREA
- RSA
- ROFA
- RUNWAY OBJECT FREE AREA

- NOTES**
1. THE EXISTING CONDITIONS SHOWN ON THIS DRAWING ARE DERIVED FROM AERIAL AND FIELD SURVEYS. PRIOR TO THE START OF THE CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL VERIFY THE EXISTENCE OF ANY DISCREPANCIES IN THE EXISTING INFORMATION AND UTILITIES ARE IDENTIFIED FROM AVAILABLE INFORMATION, BUT ARE NOT KNOWN TO BE ACCURATE OR COMPLETE. IF ACTIVE UNDOCUMENTED UNDERGROUND UTILITIES ARE ENCOUNTERED DURING CONSTRUCTION, IMMEDIATELY NOTIFY THE ENGINEER.
 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTENCE OF ALL UTILITIES AND SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ALL UTILITIES.
 3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTENCE OF ALL UTILITIES AND SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ALL UTILITIES.
 4. THE PROPOSED CONTIGUOUS LINES ARE THE RESULT OF AN ENGINEER'S JUDGMENT AND SHOULD BE CONSIDERED AS APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE EXISTENCE OF ALL UTILITIES AND SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ALL UTILITIES.
 5. REGARDING THE ENTRY OF THE AIRBORNE OR HAZARDOUS MATERIALS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ALL UTILITIES.
 6. THE CONTRACTOR SHALL NOT REMOVE EXISTING VEGETATION UNLESS REQUIRED TO PREPARE A GRADING OPERATION.



<p>CUYAHOGA COUNTY AIRPORT 28300 CURTIS WRIGHT PARKWAY RICHMOND HEIGHTS, OH 44143</p>	<p>CIVIL ENGINEERS 15000 WILSON ROAD CLEVELAND, OH 44130 CHECKED: TMS DATE: 07/27/2013</p>	<p>GRADING PLAN - 10 SAFETY AREA IMPROVEMENTS RUNWAY 6/24 CUYAHOGA COUNTY AIRPORT</p>	<p>72</p>



Ohio Department of Natural Resources

JOHN R. KASICH, GOVERNOR

JAMES ZEHRINGER, DIRECTOR

Ohio Division of Wildlife
Scott Zody, Chief
2045 Morse Rd., Bldg. G
Columbus, OH 43229-6693
Phone: (614) 265-6300

May 8, 2013

Chantil Milam
Lawhon and Associates, Inc.
1441 King Ave.
Columbus, OH 43212

Dear Ms. Milam

After reviewing the Natural Heritage Database, I find the Division of Wildlife has no records of rare or endangered species in the Cuyahoga Co. Airport area, including a one mile radius, in the City of Highlands Heights, Cuyahoga County, Ohio. We are unaware of any unique ecological sites, geologic features, animal assemblages, scenic rivers, state wildlife areas, nature preserves, parks or forests, national wildlife refuges, parks or forests, or other protected natural areas within a one mile radius of the project area.

Our inventory program has not completely surveyed Ohio and relies on information supplied by many individuals and organizations. Therefore, a lack of records for any particular area is not a statement that rare species or unique features are absent from that area. Although we inventory all types of plant communities, we only maintain records on the highest quality areas.

This letter only represents a review of rare species and natural features data within the Ohio Natural Heritage Database. It does not fulfill coordination under the National Environmental Policy Act (NEPA) or the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S. C. 661 et seq.) and does not supersede or replace the regulatory authority of any local, state or federal agency nor relieve the applicant of the obligation to comply with any local, state or federal laws or regulations.

Please contact me at 614-265-6452 if I can be of further assistance.

Sincerely,

A handwritten signature in cursive script that reads "Greg Schneider".

Greg Schneider, Administrator
Ohio Natural Heritage Program

William Ballard

From: Kessler, John <John.Kessler@dnr.state.oh.us>
Sent: Monday, May 13, 2013 11:55 AM
To: William Ballard
Cc: Tebbe, Sarah
Subject: FW: 13-183 Comments Cuyahoga Co. Airport - Mead & Hunt

Mr. Ballard,

Please see the comments from ODNR below and let me know if you have any questions. I also plan to attend the May 23 scoping meeting.

John



ODNR COMMENTS TO: Mead & Hunt; William Ballard, william.ballard@meadhunt.com

Project: Cuyahoga Co. Airport Runway 6/24 Extension – Mead & Hunt

Location: Cuyahoga County Airport; Cuyahoga, Ohio

The Ohio Department of Natural Resources (ODNR) has completed a review of the above referenced project. These comments were generated by an inter-disciplinary review within the Department. These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S.C. 661 et seq.), the National Environmental Policy Act, the Coastal Zone Management Act, Ohio Revised Code and other applicable laws and regulations. These comments are also based on ODNR's experience as the state natural resource management agency and do not supersede or replace the regulatory authority of any local, state or federal agency nor relieve the applicant of the obligation to comply with any local, state or federal laws or regulations.

Fish and Wildlife: The Division of Wildlife (DOW) has the following comments.

The project is within the range of the Indiana bat (*Myotis sodalis*), a state and federally endangered species. The following species of trees have relatively high value as potential Indiana bat roost trees: Shagbark hickory (*Carya ovata*), Shellbark hickory (*Carya laciniata*), Bitternut hickory (*Carya cordiformis*), Black ash (*Fraxinus nigra*), Green ash (*Fraxinus pennsylvanica*), White ash (*Fraxinus americana*), Shingle oak (*Quercus imbricaria*), Northern red oak (*Quercus rubra*), Slippery elm (*Ulmus rubra*), American elm (*Ulmus americana*), Eastern cottonwood (*Populus deltoides*), Silver maple (*Acer saccharinum*), Sassafras (*Sassafras albidum*), Post oak (*Quercus stellata*), and White oak (*Quercus alba*). Indiana bat habitat consists of suitable trees that include dead and dying trees with exfoliating bark, crevices, or cavities in upland areas or riparian corridors and living trees with exfoliating bark, cavities, or hollow areas formed from broken branches or tops. If suitable trees occur within the project area, these trees should be conserved. If suitable habitat occurs on the project area and trees must be cut, cutting must occur between October 1 and March 31. If suitable trees must be cut during the summer months, a net survey must be conducted between June 15 and July 31, prior to cutting. Net surveys shall incorporate either two net sites per square kilometer of project area with each net site containing a minimum of two nets used for

two consecutive nights, or one net site per kilometer of stream within the project limits with each net site containing a minimum of two nets used for two consecutive nights. If no tree removal is proposed, the project is not likely to impact this species.

The project is within the range of the piping plover (*Charadrius melodus*), a state and federally endangered bird species, and the Kirtland's warbler (*Setophaga kirtlandii*), a state and federally endangered species. These species do not nest in the state but only utilize stopover habitat as they migrate through the region. Therefore, the project is not likely to have an impact on these species.

The project is within the range of the Canada darner (*Aeshna canadensis*), a state endangered dragonfly. Wetland impacts should be avoided in order to avoid this species.

The project is within the range of the black bear (*Ursus americanus*), a state endangered species. Due to the mobility of this species, the project is not likely to impact this species.

The project is within the range of the king rail (*Rallus elegans*), a state endangered bird. A statewide survey has not been completed for this species. A lack of records does not indicate the species is absent from the area. Nests for this species are deep bowls constructed out of grass and usually hidden very well in marsh vegetation. Therefore, if this type of habitat will be impacted, construction must be avoided in this habitat during the species' nesting period of May 1 to August 1. If this type of habitat will not be impacted, the project is not likely to impact this species.

The ODNR Natural Heritage Database has no records for rare or endangered species at this project site. We are unaware of any unique ecological sites, geologic features, animal assemblages, scenic rivers, state wildlife areas, nature preserves, parks or forests, national wildlife refuges or other protected natural areas within the project area. Our inventory program does not provide a complete survey of Ohio wildlife, and relies on information supplied by many individuals and organizations. Therefore, a lack of records for any particular area is not a statement that rare species or unique features are absent from that area.

ODNR appreciates the opportunity to provide these comments. Please contact John Kessler at (614) 265-6621 if you have questions about these comments or need additional information.

John Kessler, P.E.
Environmental Services Administrator
Office of Real Estate
Ohio Department of Natural Resources
2045 Morse Rd., Columbus, OH 43229-6605
phone: 614-265-6621
email: john.kessler@dnr.state.oh.us

United States Department of the Interior



FISH AND WILDLIFE SERVICE

Ecological Services
4625 Morse Road, Suite 104
Columbus, Ohio 43230
(614) 416-8993 / FAX (614) 416-8994

April 30, 2013

Mead & Hunt, Inc.
M&H Architecture, Inc.
Attn: William Ballard
2605 Port Lansing Road
Lansing MI, 48906

TAILS: 03E15000-2013-TA-0837

Re: Cuyahoga County Airport Runway Extension

Dear Mr. Ballard,

We have received your recent correspondence requesting information about the subject proposal. There are no Federal wilderness areas, wildlife refuges or designated critical habitat within the vicinity of the project area. The following comments and recommendations will assist you in fulfilling the requirements for consultation under Section 7 of the Endangered Species Act of 1973, as amended (ESA).

The Service recommends that proposed developments avoid and minimize water quality impacts and impacts to high quality fish and wildlife habitat (e.g., forests, streams, wetlands). Additionally, natural buffers around streams and wetlands should be preserved to enhance beneficial functions. If streams or wetlands will be impacted, the Corps of Engineers should be contacted to determine whether a Clean Water Act section 404 permit is required. Best management practices should be used to minimize erosion, especially on slopes. All disturbed areas should be mulched and revegetated with native plant species. Prevention of non-native, invasive plant establishment is critical in maintaining high quality habitats.

ENDANGERED SPECIES COMMENTS: All projects in the State of Ohio lie within the range of the **Indiana bat** (*Myotis sodalis*), a federally listed endangered species. Since first listed as endangered in 1967, their population has declined by nearly 60%. Several factors have contributed to the decline of the Indiana bat, including the loss and degradation of suitable hibernacula, human disturbance during hibernation, pesticides, and the loss and degradation of forested habitat, particularly stands of large, mature trees. Fragmentation of forest habitat may also contribute to declines. During winter, Indiana bats hibernate in caves and abandoned mines. Summer habitat requirements for the species are not well defined but the following are considered important:

- (1) dead or live trees and snags with peeling or exfoliating bark, split tree trunk and/or branches, or cavities, which may be used as maternity roost areas;
- (2) live trees (such as shagbark hickory and oaks) which have exfoliating bark;
- (3) stream corridors, riparian areas, and upland woodlots which provide forage sites.

Should habitat exhibiting the characteristics described above be present at the proposed project site, we recommend that they, as well as surrounding trees, be saved wherever possible. However, if these trees cannot be avoided, they should only be cut between October 1 and March 31. If implementation of the seasonal tree cutting restriction is not possible, summer surveys should be conducted to document the presence or likely absence of the Indiana bat within the project area during the summer. The survey must be conducted by an approved surveyor and be designed and conducted in coordination with the Endangered Species Coordinator for this office. Summer surveys must be conducted between May 15 and August 15, when the presence of maternity colonies of Indiana bats could be detected.

If there is a Federal nexus for the project (e.g., Federal funding provided, Federal permits required to construct), no tree clearing on any portion of the parcel should occur until consultation under section 7 of the ESA, between the Service and the Federal action agency, is completed. We recommend that the Federal action agency submit a determination of effects to this office, relative to the Indiana bat, for our review and concurrence.

Due to the project type, size, and location, we do not anticipate adverse effects to any other federally endangered, threatened, proposed, or candidate species. Should the project design change, or during the term of this action, additional information on listed or proposed species or their critical habitat become available, or if new information reveals effects of the action that were not previously considered, consultation with the Service should be initiated to assess any potential impacts.

Due to declining budgets, smaller staff and competing priorities, we are unable to attend the May 23, 2013, agency scoping meeting.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (48 Stat. 401, as amended; 16 U.S.C. 661 et seq.), the Endangered Species Act of 1973 (ESA), as amended, and are consistent with the intent of the National Environmental Policy Act of 1969 and the U. S. Fish and Wildlife Service's Mitigation Policy. This letter provides technical assistance only and does not serve as a completed section 7 consultation document.

Sincerely,



Mary Knapp, Ph.D.
Field Supervisor



U.S. Department
of Transportation
**Federal Aviation
Administration**

Detroit Airports District Office
Metro Airport Center
11677 South Wayne Road, Ste. 107
Romulus, MI 48174

February 17, 2015

Dr. Mary Knapp, Field Supervisor
U.S. Fish and Wildlife Service
Columbus Field Office
4625 Morse Road, Suite 104
Columbus, OH 43230

Determination of Effects for the Proposed Cuyahoga County Airport
Runway Safety Area Improvements
TAILS # 03E15000-2015-TA-0200

Dear Dr. Knapp:

The Cuyahoga County Airport (CGF or Airport), located in Cuyahoga County, Ohio is proposing improvements to the runway safety area (RSA). The purpose of the project is to provide 5,500 feet of usable runway length for aircraft to takeoff in either direction and to establish compliant RSA's per FAA requirements.

The Proposed Acton will:

- Runway Safety Area (RSA) grading improvements to meet FAA design standards;
- Remove stopway at Runway 6 approach end;
- Extend Runway 6 approach end approximately 550 feet;
- Install EMAS at Runway 6 approach end;
- Displace threshold approximately 320 feet from the new Runway 6 approach end;
- Relocate Runway 24 approach end 150 feet in order to fit standard EMAS
- Install EMAS at Runway 24 approach end;
- Displace Runway 24 threshold approximately 500 feet;
- Construct new connector taxiways to accommodate Runway 6/24 relocation;
- Extension of Runway 6/24 runway and taxiway lighting facilities
- Relocation of navigational aids;
- Development of new or revised approach and departure procedures, including flight check;
- Property acquisition, fee simple and easements; and
- Tree clearing in approach areas and transitional surfaces.

The project location, major components, and ecological resources are shown on a combined exhibit, referred to as **Figure 3 – Ecological Resources Map**. Also, included in **Attachment A – Field Reconnaissance Survey for Potential Bat Habitat**.

This determination has been prepared by the Federal Aviation Administration to address any potential effects the Proposed Action may have on species protected by the Federal Endangered Species Act under the jurisdiction of the U.S. Fish and Wildlife Service (USFWS) as required by Section 7 of the ESA. The FAA is the lead Federal agency for this consultation.

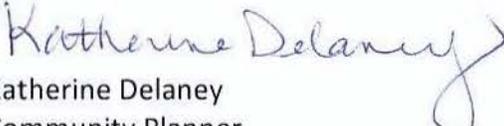
Early agency coordination with the U.S. Fish and Wildlife was completed in April 2013. A web-based agency coordination meeting was held October 17, 2013. Field work for the Proposed Action was completed April 30, 2013 – May 2, 2013. Additional reviews included a review of county records for Cuyahoga and Lake Counties and were coordinated with the Ohio Department of Natural Resources. The database review determined that four Federally threatened or endangered species are listed in Cuyahoga and Lake County.

- Indiana bat (*Myotis sodalists*)
- Snuffbox mussel (*Epioblasma triquetra*)
- Kirtland's warbler (*Dendroica kirtlandii*)
- Piping Plover (*Charadrius melodus*)

The FAA has determined that the Proposed Action will have **No Effect on the Snuffbox mussel, Kirtland's warbler, or Piping Plover** as these species are not expected to occur in the action area of the proposed project. Additionally, the ODNR Natural Heritage Program Database did not have any records of the species occurring within 1 mile of the study area. The FAA has also determined that the Proposed Action **may affect, but is not likely to adversely affect the Indiana bat**, because the ODNR Natural Heritage Program Database has no confirmed records of the species occurring within 1 mile of the study area. However, a *Field Reconnaissance Survey for Potential Bat Habitat* was completed for the surrounding area. The FAA will include cutting restrictions in our final NEPA determination.

We request concurrence from the USFWS with the determination of *No effect on the Snuffbox muscle, Kirtland's warbler, and Piping plover* and *may affect, but not likely to adversely affect determination for the Indiana bat.* If any other information is needed, please email me at Katherine.S.Delaney@faa.gov or call me at (734) 229-2958.

Sincerely,

A handwritten signature in cursive script that reads "Katherine Delaney". The signature is written in dark ink and is positioned to the right of the typed name.

Katherine Delaney
Community Planner

Cc: Cuyahoga County Airport
Mead & Hunt, Inc., Lansing



UNITED STATES DEPARTMENT OF THE INTERIOR
U.S. Fish and Wildlife Service
Ecological Services Office
4625 Morse Road, Suite 104
Columbus, Ohio 43230
(614) 416-8993 / Fax (614) 416-8994



March 3, 2015

U.S. Department of Transportation –
Federal Aviation Administration
Attn: Katherine Delaney
11677 South Wayne Road, Ste. 107
Romulus, MI 48174

TAILS# 03E15000-2015-I-0200

Reference: Cuyahoga County Airport Runway Safety Area Improvements – TAILS # 03E15000-2015-TA-0200

Dear Ms. Delaney,

We have received your recent correspondence requesting concurrence for the Cuyahoga County Airport Runway Safety Area Improvements.

A may affect, but not likely to adversely affect for Indiana bats determination was made. In agreeing to follow seasonal clearing guidelines, October 1 through March 31, we concur with you determination and project.

No affect determinations were made for the snuffbox muscle, Kirtland's warbler, and piping plover. Although we do agree with these determinations, for future projects no consultation is required when a no effect determination is made.

Should the project design change, or during the term of this action, additional information on listed or proposed species or their critical habitat become available, or if new information reveals effects of the action that were not previously considered, consultation with the Service should be initiated to assess any potential impacts.

If you have additional questions or need further assistance for your project, please contact Charlie Allen at charles_allen@fws.gov or extension 29 in this office.

Sincerely,

Dan Everson
Field Supervisor

Figure 2 - Ecological Resources Map

